

Metrolinx Lakeshore East Corridor
Community Advisory Committee RFP Meeting
Monday, October 29, 2018
Metrolinx
20 Bay St., Toronto, ON
9:00a.m. – 11:00a.m.

February 2019 Amendments/Updates are noted in blue font. Meeting summary originally shared November 2018

Approximately 20 people attended the Lakeshore East Corridor (LSE) Community Advisory Committee (CAC) RFP meeting on Monday, October 29, 2018. Attendees included CAC members, Metrolinx staff, Infrastructure Ontario staff and MPP Tabuns.

The meeting began with a safety session and a roundtable of introductions. The team referenced the agenda to guide the discussions and proceeded with an overview of the status/process of the Environmental Assessment (EA) and Projects.

For Package 1, the Transit Project Assessment Process (TPAP – an Environmental Assessment process) received notice to proceed in November 2017. This package of work is currently in the design phase and in the process of completing archeological and heritage assessments. Metrolinx is working on advancing design and assessing impacts of this project. Timing is subject to change and the current estimate is to reach 90% detailed design in Q1 2019. *Completion of detailed design is targeted for May 2019. Project Agreement development is commencing February 2019. Metrolinx provides dates and timelines as accurate as possible at any point in time – however, timing is always subject to change. GO Expansion Full Business Case was approved by our Board in December and is now under review by the Province.* Maintenance work is separate from this package and ongoing.

Metrolinx is responsible for the design process for Package 1 and the RFP will be issued as a “Build Finance” procurement. The successful bidder to the RFP will build and finance the project based on Metrolinx’s design. Infrastructure Ontario is working with Metrolinx to prepare the RFP for release.

Package 3 (*now referenced as “On Corridor” or “OnCorr”*) is a Design Build Finance Operate and Maintain (DBFOM) procurement. The successful bidder for this On-Corridor package will

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- *Attachment A: MEETING AGENDA*
 - *Attachment B: PARTICIPANT LIST*
 - *Attachment C: METROLINX RESPONSE TO THE LSE CAC’S LIST FOR INCLUSION IN THE LAKESHORE EAST CORRIDOR EXPANSION PROJECT*
 - *Attachment D: PROGRAM MILESTONES*
 - *Attachment E: COMMENTS AND RECOMMENDATIONS FROM THE CAC TO BE INCLUDED IN THE RFP(S) FOR THE METROLINX LAKESHORE EAST CORRIDOR EXPANSION PROJECT*

be responsible for all phases of the DBFOM. This package of work covers the entire GO Transit network (i.e. noise mitigation, fleet strategy, and electrification). Timing is subject to change: current RFP release is Q1 2019.

RFQ/RFP process: RFQ process qualifies bidding teams to proceed to the RFP process. After the RFP is released, approximately 4 to 6 months is required for bidders to enter their submissions and an estimated additional 5 months is needed to award the contract to the successful proponent. The [Request for Qualifications \(RFQ\) for Package 1 Lakeshore East West Expansion Project](#) was issued in March 2018 and Metrolinx shortlisted 5 RFQ proponents in November 2018. Request for Proposals (RFP) will then be released to the shortlisted proponents - currently targeting spring 2019. After Metrolinx receives bids, they will complete commercial evaluations, which take roughly 5-6 months. Metrolinx will then negotiate with the preferred proponent during this time to achieve both commercial and financial close (commercial close = getting Project Co. ready to go / financial close = closing the budget). For example, if we assume RFP is awarded to the preferred proponent in October/November 2019, construction would begin roughly in January 2020.

Noise wall installation work remains in the scope of Package 3 (OnCorr). At this stage in the procurement process, Metrolinx is unable to advance noise wall installation to Package 1. Design and procurement has progressed too far and adding a new project component would result in significant delays and would also delay Package 3 (OnCorr). Package 3 (OnCorr) RFP release date is currently targeting late 2019.

Metrolinx and Infrastructure Ontario reviewed the CAC's RFP list prior to the meeting. Many of the requests and concerns were identified as areas already covered in the EA. The details and specificity of the RFP list will rely on the contractor that comes on board to provide their mitigation plans - meaning many of the items will require further investigation. The RFPs will consider areas such as EA required noise walls, construction noise mitigation, as well as noise and vibration monitoring. [CAC raised that there seems to be a gap between what was tested and what is experienced in the homes \(i.e. homes along a ravine were not tested for noise impact because they don't back directly onto a track, but they will experience an increase in noise because of trumpeting that occurs down length of ravine. Saulter and Strange Streets are similar examples - these appear to be industrial areas, though there are residential homes on both Saulter and Strange that back directly onto portions of the track that have no planned noise mitigation\).](#) Metrolinx's mandate is to increase transit service to the region and there is a balance between meeting the mandate and resident concerns.

Tree removals [on private/public property \(e.g. not Metrolinx property\)](#) must be approved through the City of Toronto permit process and tree replanting is completed by Metrolinx. Metrolinx cannot remove any trees [on private/public property](#) without adhering to the City's by-law for tree removals. For trees on private property, tree removal will require a negotiation process directly with the owner of the property. Arborists look at trees based on the footprint of the construction area and are identified as protected or removed. The Arborist report will form part of the background data to the RFP - both documents are commercially sensitive. [Metrolinx will share the arborist report with the CAC once finalized and consult with CAC](#)

regarding public realm improvements, tree removals and replanting. Metrolinx will only remove trees to build required infrastructure.

The design process can determine if property takings are required or if property impacts will occur (i.e. subsurface easements). Metrolinx will send a notice once the properties are identified and a thorough negotiation process will ensue with experts in Property and Realty. Only the owners impacted will be directly contacted. Property requirements are being finalized as the majority of design has reached 90%. Owners to be contacted by Q1 2019 via email, notice or phone call.

Metrolinx is still working with our consultants to complete the required cultural heritage assessments, including the Riverdale Heritage District. Metrolinx received and reviewed the draft heritage impacts assessment reports and have asked the consultant to go back and do a little extra work to address indirect impacts, as well as make specific recommendations for mitigation. The extra work includes reviewing the list of properties of concern the CAC forwarded - Metrolinx will provide more information as the reports are updated.

MPP Peter Tabuns mentioned that at the Treasury Board meeting, he can speak for neighbourhoods in his riding that are along the tracks and make the request for additional funding for seamless noise mitigation.

Next Steps

- Metrolinx Action: Provide information on the RFP list noting which items would be considered for review within the different packages of work.
- Metrolinx Action: Provide an overview of timing and what is involved in each package.
- CAC Action: Send a list of properties of concern to Metrolinx.
- On January 8, Metrolinx sent the CAC the GO Transit/MOE Draft Protocol for Noise and Vibration Assessment. At the time, the draft protocol was created as a working document to be updated as needed. However, there have been no updates to the protocol since the current version.

ATTACHMENT A: MEETING AGENDA

Lakeshore East Corridor Community Advisory Committee

Date: Oct. 29, 2018
Time: 9:00 a.m. - 11:00 a.m.
Location: 20 Bay Street

Agenda Items:

1. Metrolinx Environmental Assessment team and Project team to provide brief overview/response
2. Discuss submission
3. Guidelines for contractors
4. Notification of construction activities
5. Monitoring during construction
6. Specific areas (e.g. residential dwellings and parks) along the rail corridor that residents have identified as areas of high concern

ATTACHMENT B: PARTICIPANT LIST

CAC Members

Yanina Espinoza, CHAIR - Old Riverdale Group
Lynne Patterson, VICE CHAIR - Riverside Community Group
Shelley Kline - Riverdale South Community Group
Darcie Hogan - Riverdale South Community Group
Gail Mason - Riverdale South Community Group
Joan Blake - Riverdale South Community Group
Rick Longford - Old Riverdale Group
Emily Harris - Lakeshore East Rail Coalition

Metrolinx

Rawle Agard - Manager (A), Community Relations, LSE/USRC
Mirjana Osojnicky - Manager, Environmental Programs and Assessment
Melissa Dagenais - Project Manager, Environmental Programs and Assessment
Michael Johnston - Senior Manager, RER-AFP Early Works
Nima Khajavi - Project Manager, RER- AFP Early Works
Teresa Ko - Communications Specialist, LSE/USRC
Carmen Rapati - Community Relations, LSE

Infrastructure Ontario

Rodrigo Marcellino, Project Coordinator, Project Delivery, Rail Implementation Team
Jonce Tolovski, Project Manager, Project Delivery, Rail Implementation Team

Stakeholders

MPP Peter Tabuns – Toronto/Danforth

ATTACHMENT C: METROLINX RESPONSE TO THE LSE CAC’S LIST FOR INCLUSION IN THE LAKESHORE EAST CORRIDOR EXPANSION PROJECT

Metrolinx Response to the LSE CAC’s List for Inclusion in the Lakeshore East Corridor Expansion Project

Introduction

On September 15, 2018, the LSE CAC submitted a list of items for Metrolinx to consider for inclusion in the RFP’s for the LSE GO Expansion Project Package 1, 2 and 3. Metrolinx appreciates the CAC’s effort in developing this thorough list. On October 29, Metrolinx staff and CAC members met to discuss this list. After review, Metrolinx’s overall position is that these items can be considered as the project continues to advance (e.g. no items were identified as impossible; no “red flags”). To provide a written response to the CAC, below is a series of tables which include the CAC’s original item and Metrolinx’s response to each item. As the LSE GO Expansion Project is currently at an early stage in the project life cycle (e.g. procurement and design phase), these details are subject to change as the project unfolds. Metrolinx is committed to continuing to work with the CAC and provide updates as project details become clearer.

Section 1: List of Guidelines for Contractor(s) to be included in the PA or Service Level Agreement:

CAC Item #1	Metrolinx Response
During construction, Contractor(s) will: 1.1 Work with Metrolinx to schedule significantly noisy and disruptive maintenance and construction work during the day (700h to 2300h) and avoid night work (2300h to 700h) wherever possible. This may require some minor service disruption during off-peak times (e.g. Sunday afternoons or non-rush-hour times during the week).	<ul style="list-style-type: none">• Maintenance and Construction will be scheduled during the day where possible, however night work should be anticipated.• Advance notice of nighttime work will be provided.• Metrolinx will develop a protocol for noise identification, messaging and mitigation measures that will be incorporated into Pkg 1, Pkg. 3 and Maintenance work.
1.2 Install temporary noise walls to reduce impact on neighbourhoods. Please refer to EPR Section 5.4 on Noise and Vibration recommendations, specifically in Table 8-1 (p. 142), extracted below:	<ul style="list-style-type: none">• EA Commitment• Project Co. will develop a Noise and Vibration Plan to ensure construction works stay within

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<p>Noise and Vibration</p>	<p><u>Detailed Design</u></p> <ul style="list-style-type: none"> • Develop a Construction Noise Management Plan prior to construction. The plan will include a complaint response protocol. See Section 5.4.3.1 for examples of what the plan may include. • Develop a Construction Vibration Mitigation and Monitoring Plan prior to construction. The plan will include a complaint response protocol. See Section 5.4.3.2 for examples of what the plan may include. • Complete a more detailed review of the individual effect of noise barriers during Detailed Design. • Barrier height and effectiveness will be reviewed during the Detailed Design, as the detailed grading information is required to accurately calculate the final effectiveness of the barrier. • Follow next steps in identifying what type of noise mitigation will be implemented and where, including: <ul style="list-style-type: none"> ▪ Further analysis of the noise mitigation options will be undertaken to establish what types of mitigation will be implemented and where. This will include further consideration of the administrative, operational, economic and technical feasibility as per the Protocol. ▪ Metrolinx will carry out additional public engagement once Detailed Design has progressed and updated analysis results are available. • At this time, the recommended vibration mitigation is identified as the use of ballast mats, however further analysis of vibration mitigation options will be undertaken during Detailed Design to establish what types of mitigation will be implemented. • A review of the vibration assessment will be undertaken during Detailed Design and vibration 	<p>required daytime and nighttime noise parameters. As part of its means and methods, Project Co. may choose to install temporary noise walls.</p> <ul style="list-style-type: none"> • Will be considered for Pkg. 1 and Pkg. 3*
<p>measurements will be completed for new infrastructure at relevant representative locations, as well as a reasonable number of additional representative receptor locations.</p>		
<p>1.3 Shield lights used on site during night work from nearby residences. Direct lights away from residents prior to turning them on and always use barn doors.</p>		<ul style="list-style-type: none"> • Will be considered for Pkg. 1 and Pkg. 3*
<p>1.4 Ensure that the Superintendent on site is available for calls from Metrolinx at all times</p>		<ul style="list-style-type: none"> • Will be considered for Pkg. 1 and Pkg. 3 once Project Co. is on board*
<p>1.5 Ensure that Metrolinx has the power to call a halt to all work due to complaints from the 24-hour complaint line.</p>		<ul style="list-style-type: none"> • Metrolinx will develop a protocol for complaint investigation (e.g. noise, vibration, dust, etc.), messaging and mitigation measures that will be incorporated into Pkg 1. & Pkg. 3 • Will be considered for Pkg. 1 and Pkg. 3*
<p>1.6 Wherever possible, all movement of personnel, materials and equipment to the site is by rail or via the Metrolinx right-of-way; no residential streets are to be used if can be avoided.</p>		<ul style="list-style-type: none"> • Use of residential streets will be avoided where possible. • Advance notification of use (e.g. residential streets) will be provided. • Metrolinx will develop a protocol for complaint investigation (e.g. noise, vibration, dust, access to corridor, etc.), messaging and mitigation measures that will be incorporated into Pkg 1. & Pkg. 3

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1.7 Communicate by walkie-talkie (no loudspeakers in residential neighbourhoods).	<ul style="list-style-type: none"> • Will be considered for Pkg. 1 and Pkg. 3*
1.8 Limit noise of vehicles backing up during night work.	<ul style="list-style-type: none"> • Will be considered for Pkg. 1 and Pkg. 3* • MX is currently implementing use of low frequency back up vehicles for some of our maintenance activities and are currently investigating the use for future construction contracts (Package 1 and 3) as a part of Project Co's noise and vibration mitigation plans when on board.
1.9 Not allow large equipment or trucks to idle on residential streets.	<ul style="list-style-type: none"> • Metrolinx will develop a protocol for complaint investigation (e.g. noise, vibration, dust, access to corridor, etc.), messaging and mitigation measures that will be incorporated into Pkg 1. & Pkg. 3 • Project Co. is responsible for following Municipal bylaw for idling because they need to follow applicable law
1.10 Minimize clearing of vegetation during after-hours, due to noise caused by backhoes, diggers, and chomping of trees, etc.	<ul style="list-style-type: none"> • Will be considered for Pkg. 1 and Pkg. 3* • Advance notice for night work will be provided • Metrolinx will work as much as possible during the day and limit the amount of work that takes place at night. We must keep in mind how busy this section of the rail corridor is today which will limit the flexibility when certain work can take place
1.11 Use general noise control measures during construction. All construction equipment to comply with MOECC NPC-115	<ul style="list-style-type: none"> • EA commitment • Will be considered for Pkg. 1 and

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<p>guidelines. (The Contract documents should contain a provision that any initial noise complaint will trigger verification that the general noise control measures agreed to be in effect. (extracted from Appendix G – Noise and Vibration Modelling Report (LSW), RWDI AIR Inc., Rev. 5, page xi, (9/20/17))). In the presence of persistent complaints, and subject to the results of a field investigation, the contractor shall explore alternative noise control measures with Metrolinx.</p>	<p>Pkg. 3*</p> <ul style="list-style-type: none"> • Metrolinx will develop a protocol for complaint investigation (e.g. noise, vibration, dust, etc.), messaging and mitigation measures that will be incorporated into Pkg 1. & Pkg. 3
<p>1.12 As part of the site orientation program for all trades, ensure trades know how residents can submit a noise complaint.</p>	<ul style="list-style-type: none"> • Will be considered for Pkg. 1 and Pkg. 3* • Metrolinx will develop a protocol for complaint investigation (e.g. noise, vibration, dust, etc.), messaging and mitigation measures that will be incorporated into Pkg 1. & Pkg. 3
<p>1.13 Ensure that the site orientation program for all trades includes guidance on being respectful of adjacent residential properties.</p>	<ul style="list-style-type: none"> • Will be considered for Pkg. 1 and Pkg. 3* • This will be reinforced at regular progress meetings with the project delivery team and Project Co.
<p>1.14 Ensure that crews can only work where the work has been scheduled. If crews show up somewhere to work where it is not scheduled and the neighbours don't have proper notice, the crews are to be shut down, triggered by neighbourhood complaints to 24/7 hotline.</p>	<ul style="list-style-type: none"> • Will be considered for Pkg. 1 and Pkg. 3* • Metrolinx will develop a protocol for complaint investigation (e.g. noise, vibration, dust, etc.), messaging and mitigation measures that will be incorporated into Pkg 1. & Pkg. 3 • Project Co. must demonstrate that any unexpected work carried out must meet emergency work criteria and is a part of the overall program's critical path to proceed. Community relations will follow up with impacted communities should this be the

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	case.		
1.15 Avoid construction adjacent to city parks between <u>May and September</u> , where feasible, to minimize construction-related disturbance (as per the EPR, 4.1.5).	<ul style="list-style-type: none"> • EA commitment • Will be considered for Pkg. 1 and Pkg. 3* 		
1.16 Create and implement mitigation strategies for graffiti deterrents on any constructed sound barriers in conjunction with the City and neighbourhoods throughout the rail corridor.	<ul style="list-style-type: none"> • Will be considered for Pkg. 3* 		
1.17 Develop and implement an air quality plan, as per recommendation in ERP in Table 8-1 recommended Air Quality as outlined in Table 8.1 Summary (p. 142) extracted below:	<ul style="list-style-type: none"> • EA commitment • Will be considered for Pkg. 1 and Pkg. 3* 		
<table border="1"> <tr> <td style="width: 15%; text-align: center;">Air Quality</td> <td> <p><u>Detailed Design</u></p> <ul style="list-style-type: none"> • Develop a Dust Control Plan for implementation during construction. The dust control measures will conform to recognized standard specifications such as Ontario Provincial Standards Specification (OPSS) and <i>Best Practices for the Reduction of Air Emissions from Construction and Demolition Activities</i> (March 2005), as practical. <p><u>Construction</u></p> <ul style="list-style-type: none"> • Minimize by-products of combustion (NOx, CO, VOCs, and PM) from trucks or other construction equipment by ensuring that any diesel equipment complies with the latest emission standards (Tier 3 or Tier 4). • Minimize dust resulting from construction activities by watering or applying other dust suppressants, covering up stockpiles, reducing travel speeds for heavy vehicles, minimizing haul distances, and efficiently staging construction activities. • Further mitigate potential impacts associated with emissions from construction equipment: <ul style="list-style-type: none"> • Equipment will be kept in good operating condition; • Equipment idling time will be minimized; and • Stationary equipment (e.g., generators, compressors) will be located as far away from sensitive receptors as practical. </td> </tr> </table>	Air Quality	<p><u>Detailed Design</u></p> <ul style="list-style-type: none"> • Develop a Dust Control Plan for implementation during construction. The dust control measures will conform to recognized standard specifications such as Ontario Provincial Standards Specification (OPSS) and <i>Best Practices for the Reduction of Air Emissions from Construction and Demolition Activities</i> (March 2005), as practical. <p><u>Construction</u></p> <ul style="list-style-type: none"> • Minimize by-products of combustion (NOx, CO, VOCs, and PM) from trucks or other construction equipment by ensuring that any diesel equipment complies with the latest emission standards (Tier 3 or Tier 4). • Minimize dust resulting from construction activities by watering or applying other dust suppressants, covering up stockpiles, reducing travel speeds for heavy vehicles, minimizing haul distances, and efficiently staging construction activities. • Further mitigate potential impacts associated with emissions from construction equipment: <ul style="list-style-type: none"> • Equipment will be kept in good operating condition; • Equipment idling time will be minimized; and • Stationary equipment (e.g., generators, compressors) will be located as far away from sensitive receptors as practical. 	
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CAC Item #2	Metrolinx Response
<p>2. With respect to notification of construction activities, Metrolinx will:</p> <p style="padding-left: 40px;">2.1 Include the 24/7 complaint line in all communications, websites and public notices.</p>	<ul style="list-style-type: none"> • Metrolinx is exploring opportunities to provide a 24 hour hotline on Pkg. 1, Pkg. 3*
<p>2.2 Inform residents at all addresses within 500m of the planned construction two (2) weeks prior to construction starting. Notices to be distributed in writing via Canada Post mailers, and to include:</p> <p style="padding-left: 40px;">a. when work will begin within 200 metres of homes</p> <p style="padding-left: 40px;">b. what type of work is to be done</p> <p style="padding-left: 40px;">c. scheduled duration and hours of work,</p>	<ul style="list-style-type: none"> • Advance notification of construction will be provided. • Zone of influence will be confirmed. • Will be considered for Pkg. 1 and Pkg. 3*

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<p>highlighting any work to be done at night</p> <p>d. any planned noise, vibration, use of lights</p>	
<p>2.3 When construction occurs with no advance notification, Metrolinx will provide CAC committee, within 48 hours of occurrence, a detailed description of:</p> <p>a. type of work completed</p> <p>b. copy of city permit</p> <p>c. explanation on why community was not given (2) weeks' notice</p>	<ul style="list-style-type: none"> • Advance notification of construction will be provided. • Will be considered for Pkg. 1 and Pkg. 3* • Metrolinx will develop a protocol for complaint investigation (e.g. noise, vibration, dust, access to corridor, etc.), messaging and mitigation measures that will be incorporated into Pkg 1. & Pkg. 3 • see bullet point in section 1.14
<p>2.4 Posted large (2m x 3m), highly-visible notices of construction, which includes the 24/7 complaint line to Metrolinx, in public areas (e.g. along streets, in parks) every 100m. Headings of the announcement need to be legible from at least 5m away and the details need to be clear from 2m away.</p>	<ul style="list-style-type: none"> • Advance notification of construction will be provided, including use of appropriate signage. • Will be considered for Pkg. 1 and Pkg. 3*
<p>2.5 Distribute a quarterly calendar (similar to the Toronto Garbage Collection Calendar) to each address within a 200m radius that identifies, at a high level, when and what type of construction activity is planned for the next three months. The calendar is to be made available online as well.</p>	<ul style="list-style-type: none"> • Advance notification of construction will be provided, including use of online tools. • Zone of influence will be determined. • Will be considered for Pkg. 1 and Pkg. 3*
<p>2.6 Provide quarterly written updates (distributed via Canada Post) to all addresses within a 200m radius on project progress (e.g. is it on schedule or delayed, how noise complaints have been handled)</p>	<ul style="list-style-type: none"> • Project updates will be provided, including use of traditional distribution via newsletters, as well as online tools. • Zone of influence will be determined. • Will be considered for Pkg. 1 and Pkg. 3*
<p>2.7 Institute a process for penalizing contractors who do not follow mitigation protocols after specific amount of complaints.</p>	<ul style="list-style-type: none"> • This requirement is covered by the Project Agreement. • Will be considered for Pkg. 1 and

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	Pkg. 3*
To this end, the contractor(s) will need to provide Metrolinx with updates on the project schedule, as requested.	<ul style="list-style-type: none"> • This requirement is covered by the Project Agreement. • Will be considered for Pkg. 1 and Pkg. 3*

CAC Item #3	Metrolinx Response
<p>3. With respect to clearing of vegetation and trees along the LSE rail corridor, Metrolinx will:</p> <p>3.1 Ensure decisions regarding clearing of trees and vegetation along the rail corridor are made BEFORE any construction begins, and <u>NOT BEFORE the type</u> of train (hydrogen vs. electric) is selected by Metrolinx. This decision will impact the area of vegetation cleared. Any decision to remove trees must be made in agreement with the City, specifically the City Arborist and the City Department of Urban Forestry and in consultation with the appropriate chapter of the CAC and neighbourhood groups.</p>	<ul style="list-style-type: none"> • Tree and vegetation removals for Pkg. 1 will be limited to accommodating the grading works. • An arborist report will be prepared in accordance with the City of Toronto tree protection and urban forestry by-laws, which includes identification of tree protection zones. • Applications for tree removal and injury will be submitted to the appropriate City of Toronto departments (i.e. Urban Forestry and Ravines and Natural Areas)
<p>a. Trees chosen to remain will be marked, protected and monitored by these departments and Metrolinx during the construction.</p>	<ul style="list-style-type: none"> • An arborist report will be prepared in accordance with the City of Toronto tree protection by-law, which includes identification of tree protection zones.
<p>b. Any change to the status of trees must be communicated to these City departments and appropriate CACs.</p>	<ul style="list-style-type: none"> • Project Co. to obtain additional permits from the City of Toronto as a result of design changes.
<p>c. Timing of tree and vegetation removal must also be greenlighted by the appropriate Wildlife, Bird, Fish agencies as indicated in the EPR. Identified Species at Risk (SAR)</p>	<ul style="list-style-type: none"> • Tree and vegetation removals will occur during appropriate timing windows and in accordance with applicable law (i.e. Migratory Bird Convention Act, Endangered

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mitigation plans must be implemented and monitored closely during & after construction.	Species Act). <ul style="list-style-type: none"> Monitoring and mitigation will be considered for Pkg. 1 and Pkg. 3*
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CAC Item #4	Metrolinx Response
<p>4. With respect to monitoring during construction (as per EPR, 4.2 Recommended Monitoring):</p> <p>4.1 Environmental Monitors will be on-site during key construction activities (e.g., vegetation removal), as required, to ensure compliance with environmental requirements.</p>	<ul style="list-style-type: none"> EA commitment Will be considered for Pkg. 1 and Pkg. 3*
<p>4.2 On-site inspection and maintenance by an Environmental Monitor will be undertaken on a regular basis (e.g., monthly) or as required (e.g., following storm events) over the course of construction to ensure effectiveness of erosion and sediment control measures and protective fencing. On-site inspection will be undertaken as required during construction by a qualified Arborist to ensure that only specified trees are removed, fencing is intact and there is no damage caused to the remaining trees and adjacent vegetation communities. Construction and/or silt fencing, will be repaired if it is damaged. Any damaged trees will be pruned through the implementation of proper arboricultural techniques, under supervision of an Arborist or Forester.</p>	<ul style="list-style-type: none"> EA commitment Will be considered for Pkg. 1 and Pkg. 3
<p>4.3 Nest searches by a qualified Biologist will be required immediately prior to vegetation removal, if construction activities are scheduled between May 1st and August 15th.</p>	<ul style="list-style-type: none"> EA commitment Will be considered for Pkg. 1 and Pkg. 3*
<p>4.4 If construction activities related to the proposed culvert extensions or bridge widenings take place between May 1st and August 15th, nest surveys will be conducted by a qualified Biologist to confirm absence of nests (of Barn Swallow or other birds protected under the MBCA) on, under or in any of these structures.</p>	<ul style="list-style-type: none"> EA commitment Will be considered for Pkg. 1 and Pkg. 3*

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<p>4.5 Restoration/compensation and/or post-construction monitoring may be required to ensure continued ecological function of natural features within or in the immediate vicinity of the rail ROW.</p>	<ul style="list-style-type: none"> • EA commitment • Will be considered for Pkg. 1 and Pkg. 3*
<p>4.6 Post-planting monitoring of restoration areas will be completed after construction. Should the plantings and/or seed mix not survive, additional seeding and/or plantings will be undertaken with additional monitoring during the growing season, as per the landscaping warranty.</p>	<ul style="list-style-type: none"> • EA commitment • Will be considered for Pkg. 1 and Pkg. 3*
<p>4.7 Additional restoration/compensation measures and/or monitoring may be required based on the results of additional surveys and consultations with the appropriate regulatory agency.</p>	<ul style="list-style-type: none"> • EA commitment • Will be considered for Pkg. 1 and Pkg. 3*

Section 2: List of Specific Areas of Concern to be Addressed through the PA

CAC Item #1	Metrolinx Response
<p>1. Riverside (west of the tracks - Eastern Avenue to south of Dundas Street East)</p> <p>1.1 The tracks are elevated several meters above ground level throughout this area. Due to the track elevation, the maximum height of 5 meters identified by Metrolinx for all new barriers will not sufficiently mitigate noise. Noise wall design, height and positioning must be considered with neighbourhood consultation.</p>	<ul style="list-style-type: none"> • Noise mitigation, where technically, economically and administratively feasible will be confirmed and considered for Pkg. 3*
<p>1.2 The overpass on Queen Street at De Grassi Street requires noise and vibration mitigation to reduce impact to surrounding residents, businesses, pedestrians and park goers.</p>	<ul style="list-style-type: none"> • Noise and vibration mitigation, where technically, economically and administratively feasible will be confirmed and considered for Pkg. 3* • In general, vibration mitigation will be provided where new infrastructure such as switches

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	<p>will be installed under new tracks.</p> <ul style="list-style-type: none"> • In general, noise mitigation will not be installed on bridges due to structural loading requirements.
<p>1.3 It is essential that Strange Street and the south end of Saulter Street have noise walls or other effective noise mitigation due to the number of residents and small commercial units which are very close to the tracks.</p>	<ul style="list-style-type: none"> • Noise mitigation, where technically, economically and administratively feasible will be confirmed and considered for Pkg. 3*
<p>1.4 Fontbonne Place at the corner of Strange Street and Queen Street is run by Sisters of St. Joseph, and is home to 18 single older women with rent geared to income. Bedrooms face the tracks (30m). The windows will need triple glazing or other methods of soundproofing.</p>	<ul style="list-style-type: none"> • Where noise walls are not feasible, alternative means of mitigation will be considered. • Additional mitigation will be subject to funding approvals
<p>1.5 Because of the historically significant structures adjacent to the tracks, the impact of vibration is a concern.</p> <ul style="list-style-type: none"> ○ Wardell Street and DeGrassi Street were some of the earliest developed on the east side of the Don River and reflect the period of development which stretched from the mid-1880s to World War 1 (e.g. 20, 22, 24, 26, 28 Wardell St. houses built approximately 1888.) These structures were not built to withstand the additional vibration that will be caused by the proposed rail service increases. ○ As per the AECOM Lakeshore East Rail Corridor Expansion Project – Environmental Project Report, Heritage Impact Assessments must be completed during detailed design for the proposed Queen Street East HCD (section 5.7.1). 	<ul style="list-style-type: none"> • The Cultural Heritage Screening Report recommended Heritage Impact Assessments (HIA) for the two identified Heritage Conservation Districts (HCD), the Designated Riverdale HCD and the Proposed Queen Street East HCD to be completed during Detailed Design, which are currently underway.

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CAC Item #2	Metrolinx Response
<p>2. Riverdale South (west side of tracks – Tiverton Ave, First Ave, West Ave; east side of tracks – Paisley Ave, McGee Street; portions of Dundas)</p> <p>2.1 Proximity of houses along Tiverton Ave are at significant risk of noise and vibration impact, specifically 15 Tiverton Ave and 17 Tiverton Ave.</p> <ul style="list-style-type: none"> ○ The distance from the rear exterior wall of the home at 15 Tiverton Ave to the ROW is 16 feet. 17 Tiverton Ave is 23 feet from the rear exterior wall to the ROW. Both homes have the kitchen to the rear of main level with bedrooms to the rear on second level. Triple pane windows or other suitable noise mitigation will be required. ○ Vibration mitigation (ballast mat) is proposed on the east side of the ROW due to the proximity of the house at the corner of Paisley Ave. 15 Tiverton Ave is just as close to the ROW on the west side and therefore is equally at-risk due to vibration resulting from increased rail service along the existing west-most track. Vibration mitigation is required. 	<ul style="list-style-type: none"> ● Noise and vibration mitigation, where technically, economically and administratively feasible will be confirmed and considered for Pkg. 3* ● In general, vibration mitigation will be provided where new infrastructure such as switches will be installed under new tracks. ● Additional mitigation will be subject to funding approvals
<p>2.2 The tracks are elevated several meters above ground level behind Tiverton Ave (Logan on the east side), Paisley Ave and McGee Street.</p> <ul style="list-style-type: none"> ○ The wheels of the trains can be seen from the ground-floor window at 17 Tiverton Ave, which means a 5-meter noise barrier wall will do little to mitigate noise on the main level, and nothing to help with mitigation on the second level of homes all along Tiverton Ave. Noise wall design, height and positioning must be considered with neighbourhood consultation. 	<ul style="list-style-type: none"> ● Noise and vibration mitigation, where technically, economically and administratively feasible will be confirmed and considered for Pkg. 3*
<p>2.3 Because of the historically significant structures adjacent to the tracks, impact of vibration is a concern.</p>	<ul style="list-style-type: none"> ● HIA for Riverdale HCD is currently underway.

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<ul style="list-style-type: none"> ○ The train line runs adjacent to the Riverdale Heritage Conservation District - Phase 1, which includes properties on First Ave East of Degrassi, West Ave and Tiverton Ave; some of the earliest developed on the east side of the Don River (source: Riverdale HCD Plan Phase 1, Catherine Nasmith Architect, 2008). As per the AECOM Lakeshore East Rail Corridor Expansion Project – Environmental Project Report, Heritage Impact Assessments must be completed for the Riverdale HCD (section 5.7.1). ○ Cultural Heritage Evaluation Reports to be completed for 15 and 17 Tiverton Ave ○ Cultural Heritage Evaluation Reports (CHER) to be completed for 6, 8 and 10 Paisley Ave and 60 and 62 McGee Street. 	<ul style="list-style-type: none"> ● Cultural Heritage Evaluation Reports (CHER) have been completed for 15 and 17 Tiverton Ave, 6, 8 and 10 Paisley Ave and 60 and 62 McGee Street.
<p>2.4 Lack of noise barrier on bridge over Dundas Street</p> <ul style="list-style-type: none"> ○ 2 Tiverton Ave (first house on North side of Dundas, east of the bridge) is 50 feet from the point where the bridge ROW begins. Lack of noise barriers on the bridge will have a significant impact on the houses along Dundas (12 homes) as well as the houses at the foot of Tiverton Ave (east and west sides). Noise mitigation along the tracks is required on the bridge. 	<ul style="list-style-type: none"> ● Noise and vibration mitigation, where technically, economically and administratively feasible will be confirmed and considered for Pkg. 3* ● In general, noise mitigation will not be installed on bridges due to structural loading requirements.

CAC Item #3	Metrolinx Response
<p>3. Jimmie Simpson Park REDESIGN for noise mitigation on Booth Avenue.</p> <p>3.1 Metrolinx consults with City Arborist, Dept. of Urban Forestry, and Parks, Forestry and Recreation, on the <u>re-design & re-purposing of Jimmie Simpson park, to give preference to natural solutions to noise mitigation</u>. Stage 1 (as part of the PA) the City to assemble a team of arborists and landscape architects to work with Metrolinx and with PF&R to assess the vegetation as part of assessing the whole layout and programming of the park, and with noise mitigation in mind.</p>	<ul style="list-style-type: none"> ● Our noise consultants have advised us, based on scientific analysis, that tree and vegetation options are not effective means of noise mitigation. ● Typically, parks and public spaces are not considered noise sensitive receptors and do not warrant noise

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<p>Ideally an environmental assessment of how noise travels along the tracks and through the park should help determine what type of re-landscaping & new planting & types of new planting is warranted.</p>	<p>mitigation in accordance with the Go Transit/MOE Draft Noise Protocol. However, at Jimmie Simpson Park, the EA determined that noise mitigation is required to mitigate noise for the residential properties (sensitive receptors) on Booth Avenue rather than mitigate noise for the park. The corridor is elevated in this area and noise mitigation was determined to be effective.</p> <ul style="list-style-type: none"> • Metrolinx is still working with its consultant on the detailed design. The arborist reports are underway and will be finalized with the design. Tree and vegetation removals within Jimmie Simpson Park will be kept to a minimum, that is, we will only be removing what is required and tree protection zones will be established to ensure that trees are protected during construction. • Once we have the arborist report completed, we will be working with the City of Toronto to obtain permits for tree removal in accordance with the municipal by-law. While the use of berms is likely not achievable because they require a considerable amount of land, we are
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	looking at all options to ensure the park is maintained during construction and for future community use.
3.2 Once that analysis is completed, and well ahead of any vegetation removal, alternative proposals for a revitalized JS Park should be developed, which might include berms of the various types used at Corktown Common and in the housing complex on Logan, just north of the underpass (images included* on pages 10 to 12). These proposals would be in the form of plans, sections and perspectives to help in visualizing 'before and after'. They should be presented to the public at large, specifically the surrounding neighbourhood and community for review and approval. After this process, a new design will be chosen and re-development of the park can begin.	<ul style="list-style-type: none"> • Please refer to the response above (3.1).
3.3 The existing park is safeguarded and kept viable throughout all stages of track construction.	<ul style="list-style-type: none"> • Tree protection zones will be established for all construction and high security fencing will be installed to secure the rail corridor. • Public use of the park will be maintained during construction.
3.4 'Construction fencing and/or silt fencing' mentioned in 'EPR Section 3.1.1.3 Mitigation' needs to be re-defined, then, along the lines of the above collaboration and, just as importantly, in terms of temporary sound mitigation during re-development.	<ul style="list-style-type: none"> • Construction fencing and/or silt fencing is not effective as it does not meet the specification for noise mitigation.
3.5 Metrolinx to follow all recommendations outlined in EPR Summary Table 8-1 for Recreational Use, Parks and Open Space.	<ul style="list-style-type: none"> • EA Commitment • Will be considered for Pkg. 1 and Pkg. 3*

CAC Item #4	Metrolinx Response
4. East of Greenwood to Main Street 4.1 Noise wall design, height and positioning must be considered with neighbourhood consultation.	<ul style="list-style-type: none"> • Noise and vibration mitigation, where technically, economically and administratively feasible will

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	<p>be confirmed and incorporated into the PA for Pkg. 3*</p> <ul style="list-style-type: none"> • In general, vibration mitigation will be provided where new infrastructure such as switches will be installed under new tracks. • In general, noise mitigation will not be installed on bridges due to structural loading requirements.
4.2 Bridges (noise walls and ballast mats required): Greenwood, Coxwell, Woodbine Ave and Main Street.	<ul style="list-style-type: none"> • Please refer to comment above (4.1).
4.3 Apartment building on Coatsworth Cres (north-west side of Coxwell): ballast mats should extend between Coxwell Ave and east of Merrill Bridge Road Park	<ul style="list-style-type: none"> • Please refer to comment above (4.1).
4.4 Merrill Bridge Road Park require noise walls	<ul style="list-style-type: none"> • Parks and public spaces are not considered noise sensitive receptors and do not warrant noise mitigation in accordance with the Go Transit/MOE Draft Noise Protocol.
4.5 Merrill Dog Park (Aldergrove Ave) requires noise walls and ballast mats (along full length of park through Woodbine Ave)	<ul style="list-style-type: none"> • In general, vibration mitigation will be provided where new infrastructure such as switches will be installed under new tracks. There is no new infrastructure identified in this area.
4.6 Wildwood Crescent Playground requires noise walls and ballast mats (along full length of park through Woodbine Ave)	<ul style="list-style-type: none"> • Please refer to responses above (4.5).
4.7 Stephenson Park and subdivision east of park require noise walls and ballast mats (along Oakcrest Ave, east of Westlake Ave, through Woodbine Ave)	<ul style="list-style-type: none"> • Please refer to responses above (4.5).

CAC Item #5	Metrolinx Response
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<p>5. Old Riverdale Neighbourhood</p> <p>5.1 A large number of houses within 30 m of the tracks are three storeys high. To provide adequate sound mitigation for the master bedrooms located on the third floor, a sound wall 7 metres in height is required.</p>	<ul style="list-style-type: none"> Noise and vibration mitigation, where technically, economically and administratively feasible will be confirmed and considered for Pkg. 3*
<p>5.2 To protect the rear side of houses along the west side of Unity Rd., including decks and back yards, a 2 metre noise wall is required across the Greenwood Avenue bridge.</p>	<ul style="list-style-type: none"> Please refer to response above (5.1). In general, noise mitigation will not be installed on bridges due to structural loading requirements.
<p>5.3 Eleven tall Spruce trees, planted at the foot of Lount Street to obscure the view of the Metrolinx signal bridge installed at that location, must be saved to preserve their original purpose.</p>	<ul style="list-style-type: none"> An Arborist Report is currently underway. Only those trees/vegetation required to accommodate the grading works will be removed.
<p>5.4 Smaller trees on the Metrolinx right-of-way immediately south of Monarch Park must be saved to preserve the size and viability of the wooded habitat at the south end of the park.</p>	<ul style="list-style-type: none"> An Arborist Report is currently underway. Only those tree/vegetation required to accommodate the grading works will be removed.
<p>5.5 Vibration mitigation is required over the foot tunnel located at the south-west corner of Monarch Park (noise wall already provided for).</p>	<ul style="list-style-type: none"> In general, vibration mitigation will be provided where new infrastructure such as switches will be installed under new tracks. There is no new infrastructure identified in this area.

CAC Item #6	Metrolinx Response
<p>6. Gainsborough Road</p> <p>6.1 École élémentaire catholique George-Étienne-Cartier: Tracks are elevated directly over the playground. Mitigation needed as sound already fills the playground. This is an excellent opportunity for a mural or other public art. It's an especially sensitive spot to make graffiti-proof. Additional foliage is needed to hide and soften what will be a large, concrete wall.</p> <p>6.2 Homes from George-Étienne-Cartier to the dead end of the</p>	<ul style="list-style-type: none"> Public realm improvements, such as anti-graffiti treatment will be incorporated into the Project Agreement for Pkg. 3

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<p>street have track elevation of approximately two metres and are two stories tall. Please ensure proper placement of noise wall to provide adequate mitigation.</p>	
<p>6.3 Please confirm that the large stand of mature oaks on Smalls Creek south of the tracks will not be disturbed.</p>	<ul style="list-style-type: none"> • An Arborist Report is currently underway. Only those trees/vegetation required to accommodate the grading works will be removed.
<p>6.4 Please confirm that there will be noise mitigation on both the north and south sides of the tracks where they pass over Smalls creek. This mitigation is essential as the sound travels down the ravine and homes next to the rail are angled so that sound will “trumpet” toward them if there was a break in mitigation.</p>	<ul style="list-style-type: none"> • Parks and public spaces are not considered noise sensitive receptors and do not warrant noise mitigation in accordance with the Go Transit/MOE Draft Noise Protocol.
<p>6.5 There is a break in the track at roughly 322 Gainsborough. This is very, very loud already and should be fixed as soon as possible (ideally with materials particularly designed to absorb noise and vibration). Currently every single wheel crashes into it and it is incredibly disruptive.</p>	<ul style="list-style-type: none"> • This sound is likely due to a track joint that will be welded within the next few months • Metrolinx Maintenance team inspected this section of track and found that the track infrastructure is well within safety standards. • Each track in this area is inspected three times a week.
<p>6.6 Please work with the neighbourhood in the replacement of approximately 30 mature trees on the edge of the right of way. These trees supply shade to the neighbourhood and supply habitat for flora and fauna.</p>	<ul style="list-style-type: none"> • Metrolinx is establishing a Vegetation Compensation Protocol for Metrolinx RER projects and vegetation that is removed will be compensated for in accordance with the provisions of this protocol.
<p>6.7 Please confirm that no foliage on Smalls Creek will be disturbed.</p>	<ul style="list-style-type: none"> • An Arborist Report is currently underway. Only those trees/vegetation required to accommodate the grading works will be removed.

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CAC Item #7	Metrolinx Response
<p>7. Williamson Park Ravine</p> <p>7.1 Please take all measures to minimize the impact of construction on the Williamson ravine.</p>	<ul style="list-style-type: none"> • Please refer to response above (item 6.7)
<p>7.2 Neighbourhood consultation required to provide input on what kind of retaining walls will be installed.</p>	<ul style="list-style-type: none"> • Metrolinx is currently consulting with the City of Toronto on the design and finishes of all public facing retaining walls.
<p>7.3 Construction access to Williamson Ravine needs to be determined with neighbourhood consultation to ensure the protection of the sensitive ravine area and plant life.</p>	<ul style="list-style-type: none"> • Construction access areas will be refined in the coming months and we will do our best to minimize impacts to the Williamson Ravine.

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ATTACHMENT D: PROGRAM MILESTONES

DELIVERY PACKAGES

Package 1: Enabling Works

- Mostly civil works projects that can be completed by 2022
- Scope includes:
 - Grading for the future fourth track on the south side between the Don River Bridge and Gerrard Street shifting to the north side between Pape Avenue and Scarborough GO Station
 - Widening of bridges at: Woodbine Avenue, Warden Avenue and Danforth Avenue
 - Widening works under the Birchmount Road Bridge
 - Danforth Station Rehabilitation
 - Retaining walls
 - Existing culvert rehabilitation and a new culvert at Small's Creek
- Timing (subject to change):
 - Detailed Design: Present - January 2019
 - RFP: Early 2019
 - Construction: TBD

DELIVERY PACKAGES

Package 2: Off-Corridor Works

- Development of new stations at Gerrard and East Harbour
- RFQ scope:
 - New station buildings or pavilions
 - Trackbed and grading
 - Platforms
 - New parking
 - Pedestrian access (tunnel or overpass)
 - Public realm improvements
- Timing:
 - RFQ: April 2018
 - RFP: TBD
 - Construction: TBD

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DELIVERY PACKAGES

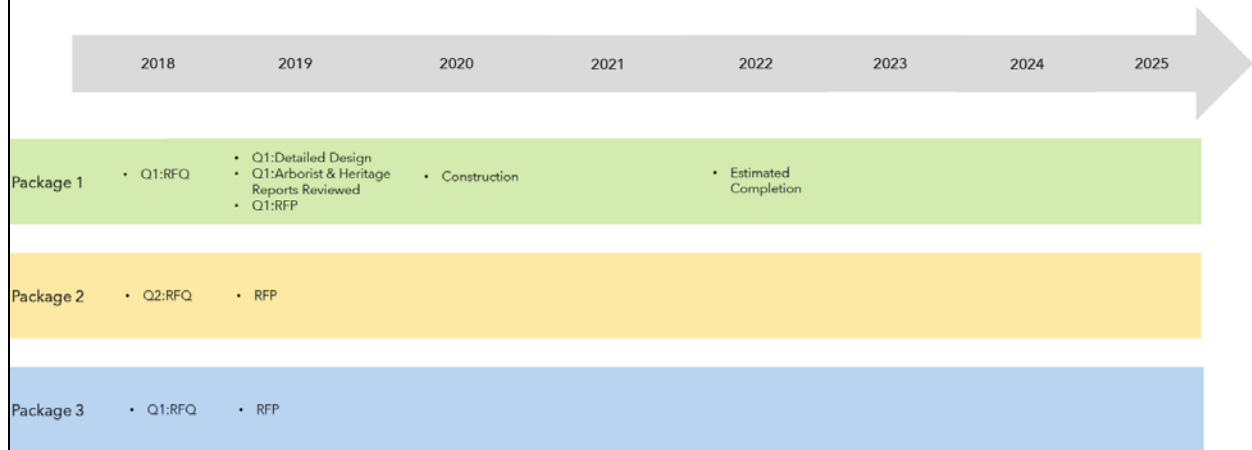
Package 3: On-Corridor Works

- GO Expansion On Corridor Project

- The RFQ outlines the scope of work required, which includes:
 - Track construction
 - New trains: new electric locomotives and electric multiple units
 - Civil works: upgrades to bridges, retaining walls, noise walls, grading, etc.
 - Union Station: changing the platform and track configurations
 - Systems: electrification infrastructure and power system upgrades
 - Operations
 - Maintenance

- Timing:
 - RFQ: March 2018
 - RFP: TBD
 - Construction: TBD

TIMELINES - MILESTONES



Notes:
 - Timelines are subject to change
 - Quarters fall within the calendar year. (Q1=Jan-Mar / Q2=Apr-June / Q3=Jul-Sep / Q4=Oct-Dec)

ATTACHMENT E: COMMENTS AND RECOMMENDATIONS FROM THE CAC TO BE INCLUDED IN THE RFP(S) FOR THE METROLINX LAKESHORE EAST CORRIDOR EXPANSION PROJECT

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**Comments and Recommendations from CAC to be included in the RFP(s) for
the Metrolinx Lakeshore East Corridor Expansion Project
Submitted by the CAC September 15, 2018**

Executive Summary

We, the Community Advisory Committee (“CAC”), have prepared the following document with input from residents adjacent to the Lakeshore East Rail Corridor who will be impacted by both the planning, design, and construction of the Regional Express Rail program (“the project”) from now until 2025, and the years that follow once the project is complete.

This document provides a list of items we request Metrolinx consider for inclusion in the RFPs Metrolinx will use to procure service providers for the project (Package 1, 2 and 3), specifically along the Lakeshore East Rail Corridor. The document is comprised of two sections:

- **Section 1:** Guidelines for contractor(s) to be included in the RFP or the Service Level Agreement(s) for the successful proponent (listed in no particular order); and
- **Section 2:** Specific areas (e.g. residential dwellings and parks) along the rail corridor that residents have identified as areas of high concern, which will require special due diligence, mitigation strategies and care during planning, design and construction of the project.

We understand Metrolinx’s need to minimize impact to service levels and ridership, and that as a result, some of these requests may not be feasible. However, we ask Metrolinx to review each item with an open mind, taking into consideration the impact multiple years of construction noise, vibration and dust caused by the enabling works, off-corridor works, and on-corridor works, and the long-term impact of significantly increased service levels will have on:

- Thousands of residents, including seniors and young families, living along the rail line;
- Structures (private dwellings, rec centres and independent commercial properties) that are culturally and historically unique to this part of the city; and
- The trees, wildlife habitats and wetlands throughout our parks and green spaces, which are integral to maintaining the vibrant communities we have today.

We ask that contractual language related to each of the items listed herein be included in the RFP or the Service Level Agreement(s) to hold the successful proponent(s) to account during project execution, whereby minimizing the impact of the RER program to surrounding residents, structures and parks as much as possible, and to follow the recommendation mitigation strategies as outlined in the EPR.

Please note, we recognize that some of these items may apply to some but not all packages of work. We look to Metrolinx to determine which RFP each item needs to be included in.

As Metrolinx has advised us that the development of the RFP(s) will be an inclusive and collaborative process involving the CAC, we plan to schedule a follow-up meeting with Metrolinx to review this list in person to clarify any areas of concern or provide rationale for our requests.

Thank you.

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Section 1: List of Guidelines for Contractor(s) to be included in the RFP or Service Level Agreement:

1. During construction, Contractor(s) will:

- 1.1 Work with Metrolinx to schedule significantly noisy and disruptive maintenance and construction work during the day (700h to 2300h) and avoid night work (2300h to 700h) wherever possible. This may require some minor service disruption during off-peak times (e.g. Sunday afternoons or non-rush-hour times during the week).
- 1.2 Install temporary noise walls to reduce impact on neighbourhoods. Please refer to EPR Section 5.4 on Noise and Vibration recommendations, specifically in Table 8-1 (p. 142), extracted below:

Noise and Vibration	<p><u>Detailed Design</u></p> <ul style="list-style-type: none"> • Develop a Construction Noise Management Plan prior to construction. The plan will include a complaint response protocol. See Section 5.4.3.1 for examples of what the plan may include. • Develop a Construction Vibration Mitigation and Monitoring Plan prior to construction. The plan will include a complaint response protocol. See Section 5.4.3.2 for examples of what the plan may include. • Complete a more detailed review of the individual effect of noise barriers during Detailed Design. • Barrier height and effectiveness will be reviewed during the Detailed Design, as the detailed grading information is required to accurately calculate the final effectiveness of the barrier. • Follow next steps in identifying what type of noise mitigation will be implemented and where, including: <ul style="list-style-type: none"> ▪ Further analysis of the noise mitigation options will be undertaken to establish what types of mitigation will be implemented and where. This will include further consideration of the administrative, operational, economic and technical feasibility as per the Protocol. ▪ Metrolinx will carry out additional public engagement once Detailed Design has progressed and updated analysis results are available. • At this time, the recommended vibration mitigation is identified as the use of ballast mats, however further analysis of vibration mitigation options will be undertaken during Detailed Design to establish what types of mitigation will be implemented. • A review of the vibration assessment will be undertaken during Detailed Design and vibration 	• N/A
	measurements will be completed for new infrastructure at relevant representative locations, as well as a reasonable number of additional representative receptor locations	

- 1.3 Shield lights used on site during night work from nearby residences. Direct lights away from residents prior to turning them on and always use barn doors.
- 1.4 Ensure that the Superintendent on site is available for calls from Metrolinx at all times
- 1.5 Ensure that Metrolinx has the power to call a halt to all work due to complaints from the 24-hour complaint line.
- 1.6 Wherever possible, all movement of personnel, materials and equipment to the site is by rail or via the Metrolinx right-of-way; no residential streets are to be used if can be avoided.
- 1.7 Communicate by walkie-talkie (no loudspeakers in residential neighbourhoods).
- 1.8 Limit noise of vehicles backing up during night work.
- 1.9 Not allow large equipment or trucks to idle on residential streets.
- 1.10 Minimize clearing of vegetation during after-hours, due to noise caused by backhoes, diggers, and chopping of trees, etc.
- 1.11 Use general noise control measures during construction. All construction equipment to comply with MOECC NPC-115 guidelines. (The Contract documents should contain a provision that any initial noise complaint will trigger verification that the general noise control measures agreed to be in effect. (extracted from Appendix G – Noise and Vibration Modelling Report (LSW), RWDI AIR Inc., Rev. 5, page xi, (9/20/17))). In the presence of persistent complaints, and subject to the results of a field investigation, the contractor shall explore alternative noise control measures with Metrolinx.
- 1.12 As part of the site orientation program for all trades, ensure trades know how residents can submit a noise complaint.

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- 1.13 Ensure that the site orientation program for all trades includes guidance on being respectful of adjacent residential properties.
- 1.14 Ensure that crews can only work where the work has been scheduled. If crews show up somewhere to work where it is not scheduled and the neighbours don't have proper notice, the crews are to be shut down, triggered by neighbourhood complaints to 24/7 hotline.
- 1.15 Avoid construction adjacent to city parks between May and September, where feasible, to minimize construction-related disturbance (as per the EPR, 4.1.5).
- 1.16 Create and implement mitigation strategies for graffiti deterrents on any constructed sound barriers in conjunction with the City and neighbourhoods throughout the rail corridor.
- 1.17 Develop and implement an air quality plan, as per recommendation in ERP in Table 8-1 recommended Air Quality as outlined in Table 8.1 Summary (p. 142) extracted below:

Air Quality	<p><u>Detailed Design</u></p> <ul style="list-style-type: none"> • Develop a Dust Control Plan for implementation during construction. The dust control measures will conform to recognized standard specifications such as Ontario Provincial Standards Specification (OPSS) and <i>Best Practices for the Reduction of Air Emissions from Construction and Demolition Activities</i> (March 2005), as practical. <p><u>Construction</u></p> <ul style="list-style-type: none"> • Minimize by-products of combustion (NOx, CO, VOCs, and PM) from trucks or other construction equipment by ensuring that any diesel equipment complies with the latest emission standards (Tier 3 or Tier 4). • Minimize dust resulting from construction activities by watering or applying other dust suppressants, covering up stockpiles, reducing travel speeds for heavy vehicles, minimizing haul distances, and efficiently staging construction activities. • Further mitigate potential impacts associated with emissions from construction equipment: <ul style="list-style-type: none"> • Equipment will be kept in good operating condition; • Equipment idling time will be minimized, and • Stationary equipment (e.g., generators, compressors) will be located as far away from sensitive receptors as practical. 	N/A
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- 2. With respect to notification of construction activities, Metrolinx will:
 - 2.1 Include the 24/7 complaint line in all communications, websites and public notices.
 - 2.2 Inform residents at all addresses within 500m of the planned construction two (2) weeks prior to construction starting. Notices to be distributed in writing via Canada Post mailers, and to include:
 - a. when work will begin within 200 metres of homes
 - b. what type of work is to be done
 - c. scheduled duration and hours of work, highlighting any work to be done at night
 - d. any planned noise, vibration, use of lights
 - 2.3 When construction occurs with no advance notification, Metrolinx will provide CAC committee, within 48 hours of occurrence, a detailed description of:
 - a. type of work completed
 - b. copy of city permit
 - c. explanation on why community was not given (2) weeks' notice
 - 2.4 Posted large (2m x 3m), highly-visible notices of construction, which includes the 24/7 complaint line to Metrolinx, in public areas (e.g. along streets, in parks) every 100m. Headings of the announcement need to be legible from at least 5m away and the details need to be clear from 2m away.
 - 2.5 Distribute a quarterly calendar (similar to the Toronto Garbage Collection Calendar) to each address within a 200m radius that identifies, at a high level, when and what type of construction activity is planned for the next three months. The calendar is to be made available online as well.

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- 2.6 Provide quarterly written updates (distributed via Canada Post) to all addresses within a 200m radius on project progress (e.g. is it on schedule or delayed, how noise complaints have been handled)
- 2.7 Institute a process for penalizing contractors who do not follow mitigation protocols after specific amount of complaints.

To this end, the contractor(s) will need to provide Metrolinx with updates on the project schedule, as requested.

3. With respect to clearing of vegetation and trees along the LSE rail corridor, Metrolinx will:

- 3.1 Ensure decisions regarding clearing of trees and vegetation along the rail corridor are made BEFORE any construction begins, and NOT BEFORE the type of train (hydrogen vs. electric) is selected by Metrolinx. This decision will impact the area of vegetation cleared. Any decision to remove trees must be made in agreement with the City, specifically the City Arborist and the City Department of Urban Forestry and in consultation with the appropriate chapter of the CAC and neighbourhood groups.
 - a. Trees chosen to remain will be marked, protected and monitored by ~~these~~ departments and Metrolinx during the construction.
 - b. Any change to the status of trees must be communicated to these City departments and appropriate CACs.
 - c. Timing of tree and vegetation removal must also be greenlighted by the appropriate Wildlife, Bird, Fish agencies as indicated in the EPR. Identified Species at Risk (SAR) mitigation plans must be implemented and monitored closely during & after construction.

4. With respect to monitoring during construction (as per EPR, 4.2 Recommended Monitoring):

- 4.1 Environmental Monitors will be on-site during key construction activities (e.g., vegetation removal), as required, to ensure compliance with environmental requirements.
- 4.2 On-site inspection and maintenance by an Environmental Monitor will be undertaken on a regular basis (e.g., monthly) or as required (e.g., following storm events) over the course of construction to ensure effectiveness of erosion and sediment control measures and protective fencing. On-site inspection will be undertaken as required during construction by a qualified Arborist to ensure that only specified trees are removed, fencing is intact and there is no damage caused to the remaining trees and adjacent vegetation communities. Construction and/or silt fencing, will be repaired if it is damaged. Any damaged trees will be pruned through the implementation of proper arboricultural techniques, under supervision of an Arborist or Forester.
- 4.3 Nest searches by a qualified Biologist will be required immediately prior to vegetation removal, if construction activities are scheduled between May 1st and August 15th.
- 4.4 If construction activities related to the proposed culvert extensions or bridge widenings take place between May 1st and August 15th, nest surveys will be conducted by a

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qualified Biologist to confirm absence of nests (of Barn Swallow or other birds protected under the MBCA) on, under or in any of these structures.

- 4.5 Restoration/compensation and/or post-construction monitoring may be required to ensure continued ecological function of natural features within or in the immediate vicinity of the rail ROW.
- 4.6 Post-planting monitoring of restoration areas will be completed after construction. Should the plantings and/or seed mix not survive, additional seeding and/or plantings will be undertaken with additional monitoring during the growing season, as per the landscaping warranty.
- 4.7 Additional restoration/compensation measures and/or monitoring may be required based on the results of additional surveys and consultations with the appropriate regulatory agency.

Section 2: List of Specific Areas of Concern to be Addressed through the RFP

1. Riverside (west of the tracks - Eastern Avenue to south of Dundas Street East)

- 1.1 The tracks are elevated several meters above ground level throughout this area. Due to the track elevation, the maximum height of 5 meters identified by Metrolinx for all new barriers will not sufficiently mitigate noise. Noise wall design, height and positioning must be considered with neighbourhood consultation.
- 1.2 The overpass on Queen Street at De Grassi Street requires noise and vibration mitigation to reduce impact to surrounding residents, businesses, pedestrians and park goers.
- 1.3 It is essential that Strange Street and the south end of Saulter Street have noise walls or other effective noise mitigation due to the number of residents and small commercial units which are very close to the tracks.
- 1.4 Fontbonne Place at the corner of Strange Street and Queen Street is run by Sisters of St. Joseph, and is home to 18 single older women with rent geared to income. Bedrooms face the tracks (30m). The windows will need triple glazing or other methods of soundproofing.
- 1.5 Because of the historically significant structures adjacent to the tracks, the impact of vibration is a concern.
 - Wardell Street and DeGrassi Street were some of the earliest developed on the east side of the Don River and reflect the period of development which stretched from the mid-1880s to World War 1 (e.g. 20, 22, 24, 26, 28 Wardell St. houses built approximately 1888.) These structures were not built to withstand the additional vibration that will be caused by the proposed rail service increases.
 - As per the AECOM Lakeshore East Rail Corridor Expansion Project – Environmental Project Report, Heritage Impact Assessments must be completed during detailed design for the proposed Queen Street East HCD (section 5.7.1).

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2. **Riverdale South (west side of tracks – Tiverton Ave, First Ave, West Ave; east side of tracks – Paisley Ave, McGee Street; portions of Dundas)**
 - 2.1 Proximity of houses along Tiverton Ave are at significant risk of noise and vibration impact, specifically 15 Tiverton Ave and 17 Tiverton Ave.
 - The distance from the rear exterior wall of the home at 15 Tiverton Ave to the ROW is 16 feet. 17 Tiverton Ave is 23 feet from the rear exterior wall to the ROW. Both homes have the kitchen to the rear of main level with bedrooms to the rear on second level. Triple pane windows or other suitable noise mitigation will be required.
 - Vibration mitigation (ballast mat) is proposed on the east side of the ROW due to the proximity of the house at the corner of Paisley Ave. 15 Tiverton Ave is just as close to the ROW on the west side and therefore is equally at-risk due to vibration resulting from increased rail service along the existing west-most track. Vibration mitigation is required.
 - 2.2 The tracks are elevated several meters above ground level behind Tiverton Ave (Logan on the east side), Paisley Ave and McGee Street.
 - The wheels of the trains can be seen from the ground-floor window at 17 Tiverton Ave, which means a 5-meter noise barrier wall will do little to mitigate noise on the main level, and nothing to help with mitigation on the second level of homes all along Tiverton Ave. Noise wall design, height and positioning must be considered with neighbourhood consultation.
 - 2.3 Because of the historically significant structures adjacent to the tracks, impact of vibration is a concern.
 - The train line runs adjacent to the Riverdale Heritage Conservation District - Phase 1, which includes properties on First Ave East of Degrassi, West Ave and Tiverton Ave; some of the earliest developed on the east side of the Don River (source: Riverdale HCD Plan Phase 1, Catherine Nasmith Architect, 2008). As per the AECOM Lakeshore East Rail Corridor Expansion Project – Environmental Project Report, Heritage Impact Assessments must be completed for the Riverdale HCD (section 5.7.1).
 - Cultural Heritage Evaluation Reports to be completed for 15 and 17 Tiverton Ave
 - Cultural Heritage Evaluation Reports (CHER) to be completed for 6, 8 and 10 Paisley Ave and 60 and 62 McGee Street.
 - 2.4 Lack of noise barrier on bridge over Dundas Street
 - 2 Tiverton Ave (first house on North side of Dundas, east of the bridge) is 50 feet from the point where the bridge ROW begins. Lack of noise barriers on the bridge will have a significant impact on the houses along Dundas (12 homes) as well as the houses at the foot of Tiverton Ave (east and west sides). Noise mitigation along the tracks is required on the bridge.
3. **Jimmie Simpson Park REDESIGN for noise mitigation on Booth Avenue.**
 - 3.1 Metrolinx consults with City Arborist, Dept. of Urban Forestry, and Parks, Forestry and Recreation, on the re-design & re-purposing of Jimmie Simpson park, to give preference to

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natural solutions to noise mitigation. Stage 1 (as part of the RFP) the City to assemble a team of arborists and landscape architects to work with Metrolinx and with PF&R to assess the vegetation as part of assessing the whole layout and programming of the park, and with noise mitigation in mind. Ideally an environmental assessment of how noise travels along the tracks and through the park should help determine what type of re-landscaping & new planting & types of new planting is warranted.

- 3.2 Once that analysis is completed, and well ahead of any vegetation removal, alternative proposals for a revitalized JS Park should be developed, which might include berms of the various types used at Corktown Common and in the housing complex on Logan, just north of the underpass (images included* on pages 10 to 12). These proposals would be in the form of plans, sections and perspectives to help in visualizing 'before and after'. They should be presented to the public at large, specifically the surrounding neighbourhood and community for review and approval. After this process, a new design will be chosen and re-development of the park can begin.
- 3.3 The existing park is safeguarded and kept viable throughout all stages of track construction.
- 3.4 'Construction fencing and/or silt fencing' mentioned in 'EPR Section 3.1.1.3 Mitigation' needs to be re-defined, then, along the lines of the above collaboration and, just as importantly, in terms of temporary sound mitigation during re-development.
- 3.5 Metrolinx to follow all recommendations outlined in EPR Summary Table 8-1 for Recreational Use, Parks and Open Space.

4. East of Greenwood to Main Street

- 4.1 Noise wall design, height and positioning must be considered with neighbourhood consultation.
- 4.2 Bridges (noise walls and ballast mats required): Greenwood, Coxwell, Woodbine Ave and Main Street.
- 4.3 Apartment building on Coatsworth Cres (north-west side of Coxwell): ballast mats should extend between Coxwell Ave and east of Merrill Bridge Road Park
- 4.4 Merrill Bridge Road Park require noise walls
- 4.5 Merrill Dog Park (Aldergrove Ave) requires noise walls and ballast mats (along full length of park through Woodbine Ave)
- 4.6 Wildwood Crescent Playground requires noise walls and ballast mats (along full length of park through Woodbine Ave)
- 4.7 Stephenson Park and subdivision east of park require noise walls and ballast mats (along Oakcrest Ave, east of Westlake Ave, through Woodbine Ave)

5. Old Riverdale Neighbourhood

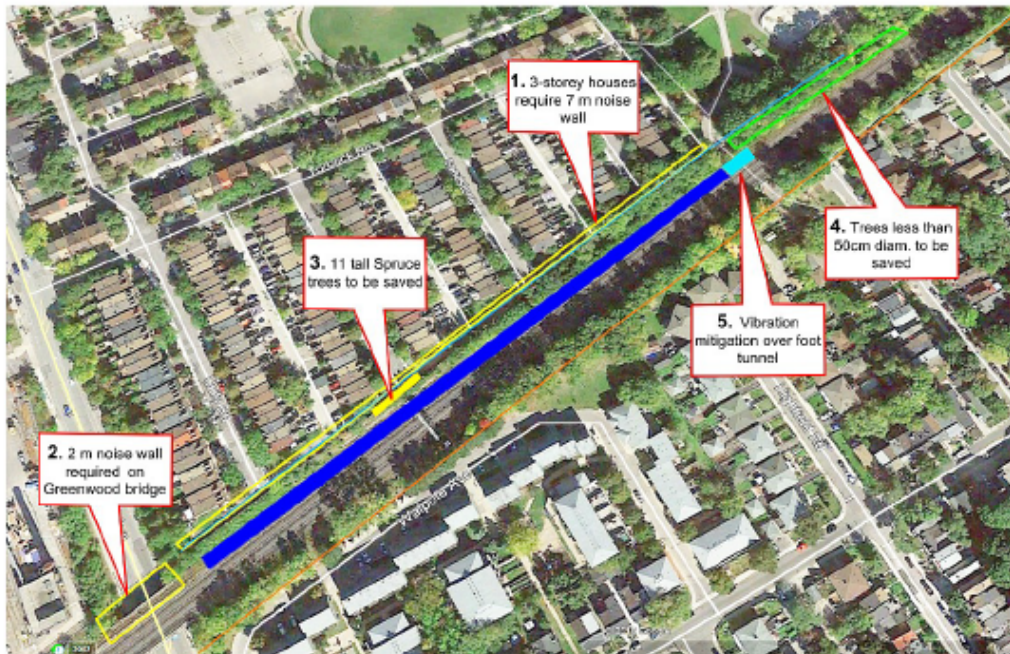
- 5.1 A large number of houses within 30 m of the tracks are three storeys high. To provide adequate sound mitigation for the master bedrooms located on the third floor, a sound wall 7 metres in height is required.

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- 5.2 To protect the rear side of houses along the west side of Unity Rd., including decks and back yards, a 2 metre noise wall is required across the Greenwood Avenue bridge.
- 5.3 Eleven tall Spruce trees, planted at the foot of Lount Street to obscure the view of the Metrolinx signal bridge installed at that location, must be saved to preserve their original purpose.
- 5.4 Smaller trees on the Metrolinx right-of-way immediately south of Monarch Park must be saved to preserve the size and viability of the wooded habitat at the south end of the park.
- 5.5 Vibration mitigation is required over the foot tunnel located at the south-west corner of Monarch Park (noise wall already provided for).

Please see image below for precise locations.

Old Riverdale Neighbourhood: Special Situations



6. Gainsborough Road

- 6.1 École élémentaire catholique George-Étienne-Cartier: Tracks are elevated directly over the playground. Mitigation needed as sound already fills the playground. This is an excellent opportunity for a mural or other public art. It's an especially sensitive spot to make graffiti-proof. Additional foliage is needed to hide and soften what will be a large, concrete wall.

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- 6.2 Homes from George-Étienne-Cartier to the dead end of the street have track elevation of approximately two metres and are two stories tall. Please ensure proper placement of noise wall to provide adequate mitigation.
- 6.3 Please confirm that the large stand of mature oaks on Smalls Creek south of the tracks will not be disturbed.
- 6.4 Please confirm that there will be noise mitigation on both the north and south sides of the tracks where they pass over Smalls creek. This mitigation is essential as the sound travels down the ravine and homes next to the rail are angled so that sound will “trumpet” toward them if there was a break in mitigation.
- 6.5 There is a break in the track at roughly 322 Gainsborough. This is very, very loud already and should be fixed as soon as possible (ideally with materials particularly designed to absorb noise and vibration). Currently every single wheel crashes into it and it is incredibly disruptive.
- 6.6 Please work with the neighbourhood in the replacement of approximately 30 mature trees on the edge of the right of way. These trees supply shade to the neighbourhood and supply habitat for flora and fauna.
- 6.7 Please confirm that no foliage on Smalls Creek will be disturbed.

7. Williamson Park Ravine

- 7.1 Please take all measures to minimize the impact of construction on the Williamson ravine.
- 7.2 Neighbourhood consultation required to provide input on what kind of retaining walls will be installed.
- 7.3 Construction access to Williamson Ravine needs to be determined with neighbourhood consultation to ensure the protection of the sensitive ravine area and plant life.

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*Images of natural sound Berms from around Corktown Common, Toronto:



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